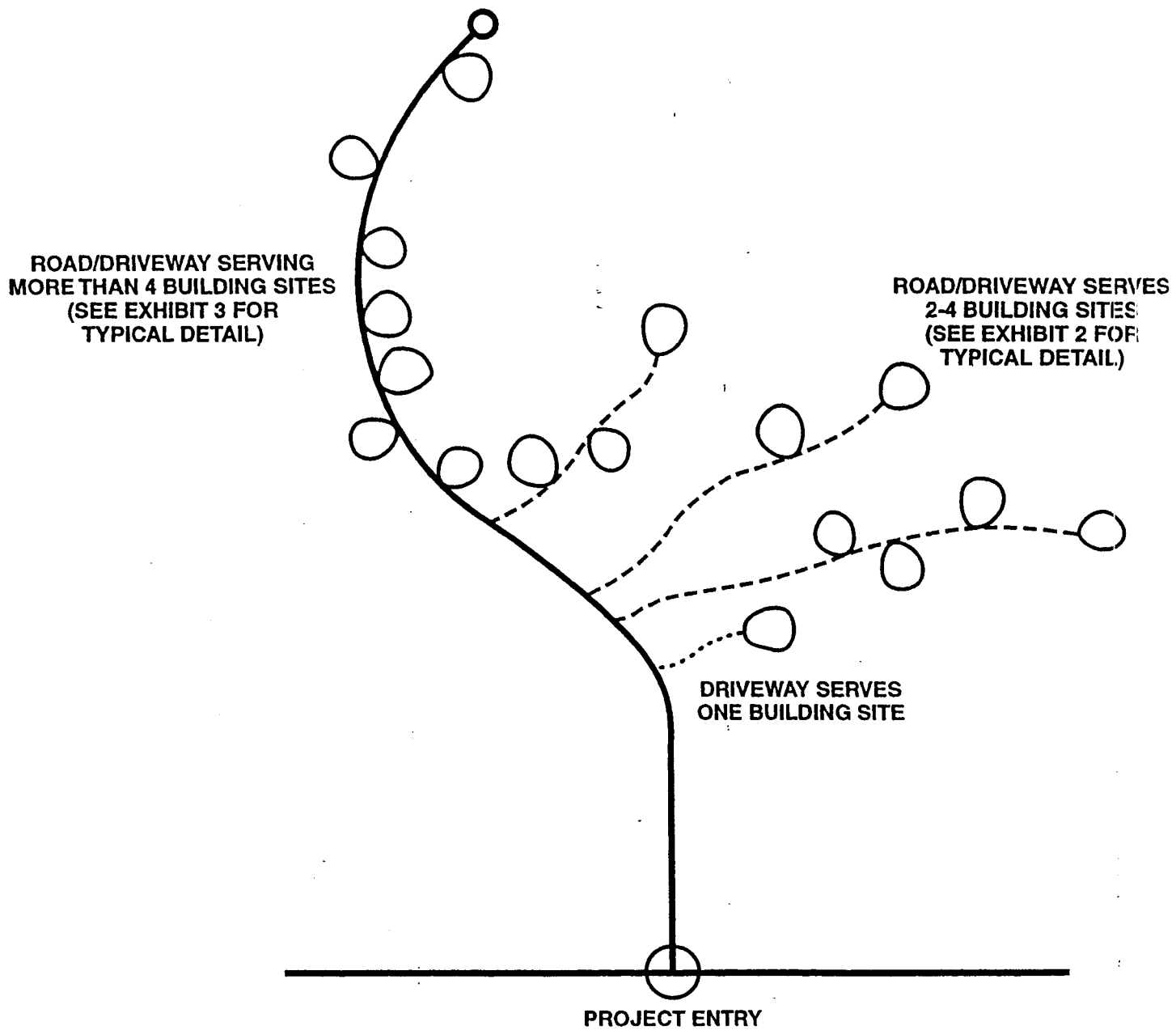


SADDLE CREEK AREA PLAN



LEGEND


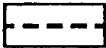
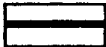
-  DRIVEWAY SERVES ONE BUILDING SITE
-  ROAD/DRIVEWAY SERVES 2-4 BUILDING SITES
-  ROAD/DRIVEWAY SERVES MORE THAN 4 BUILDING SITES

EXHIBIT 1

TYPICAL ROAD/DRIVEWAY CONDITIONS SERVING ONE AND MORE BUILDING SITES

SADDLE CREEK AREA PLAN

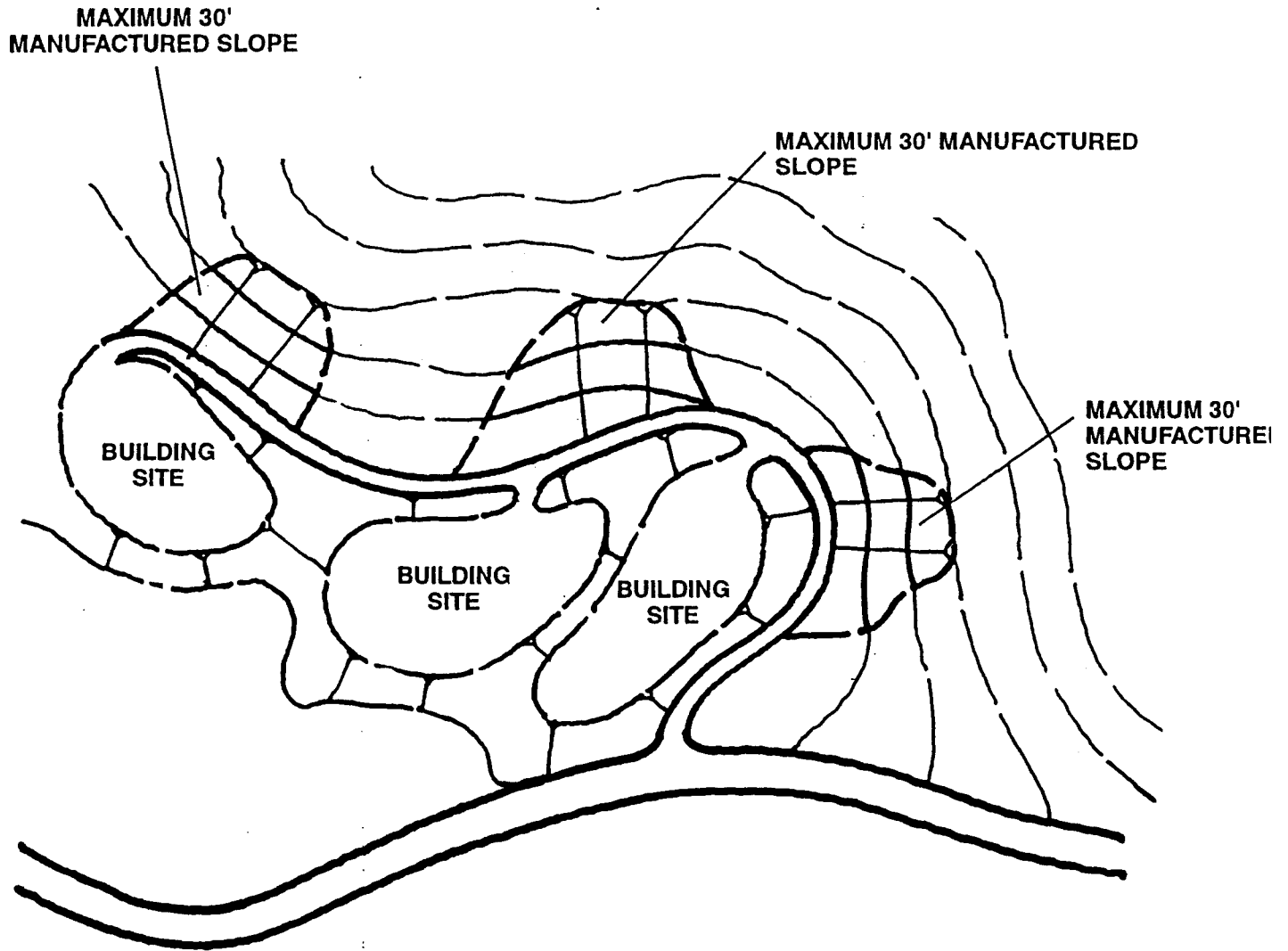


EXHIBIT 2
TYPICAL ROAD/DRIVEWAY AND SLOPE HEIGHT
CONDITIONS: ROAD/DRIVEWAY SERVING
2 OR MORE BUILDING SITES

SADDLE CREEK AREA PLAN

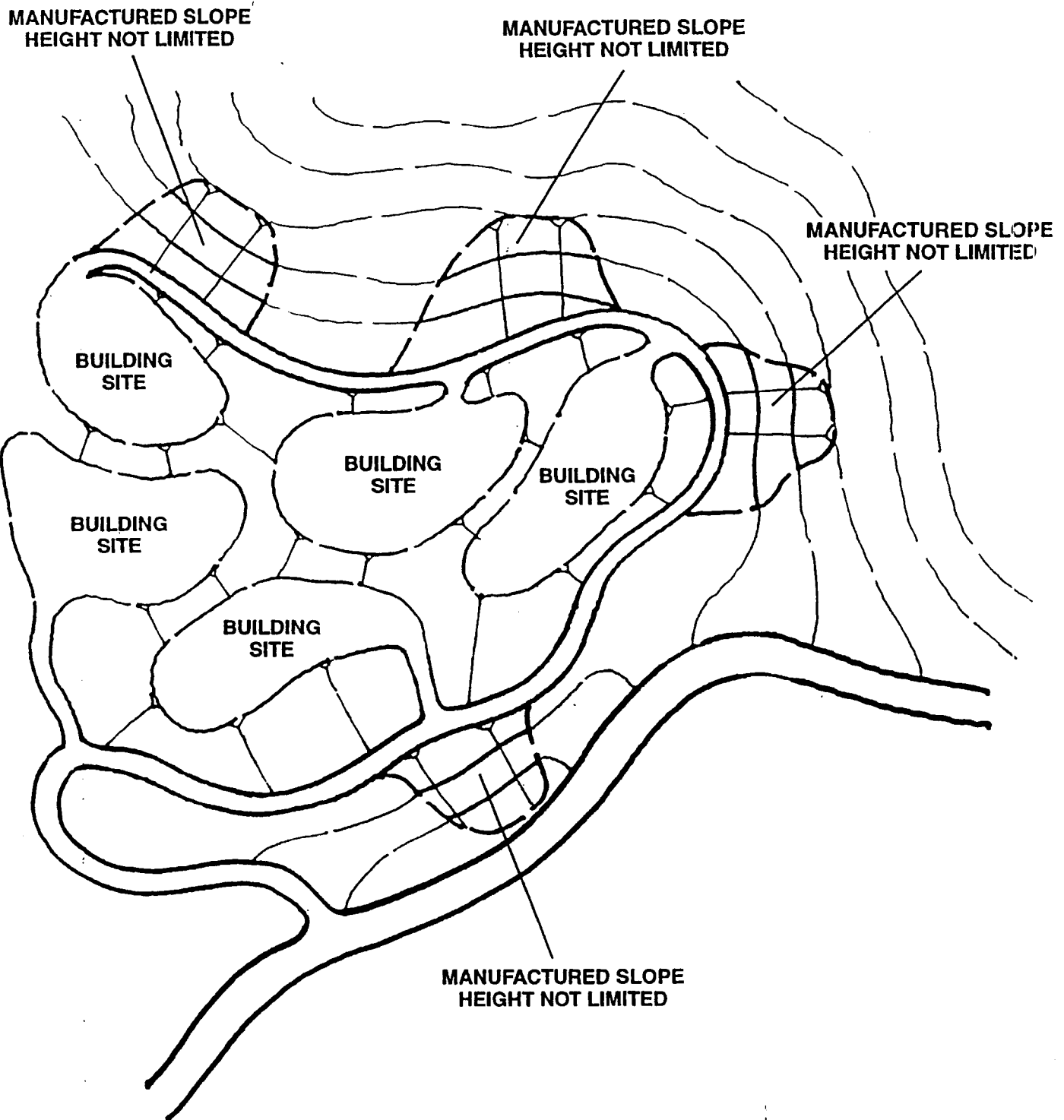
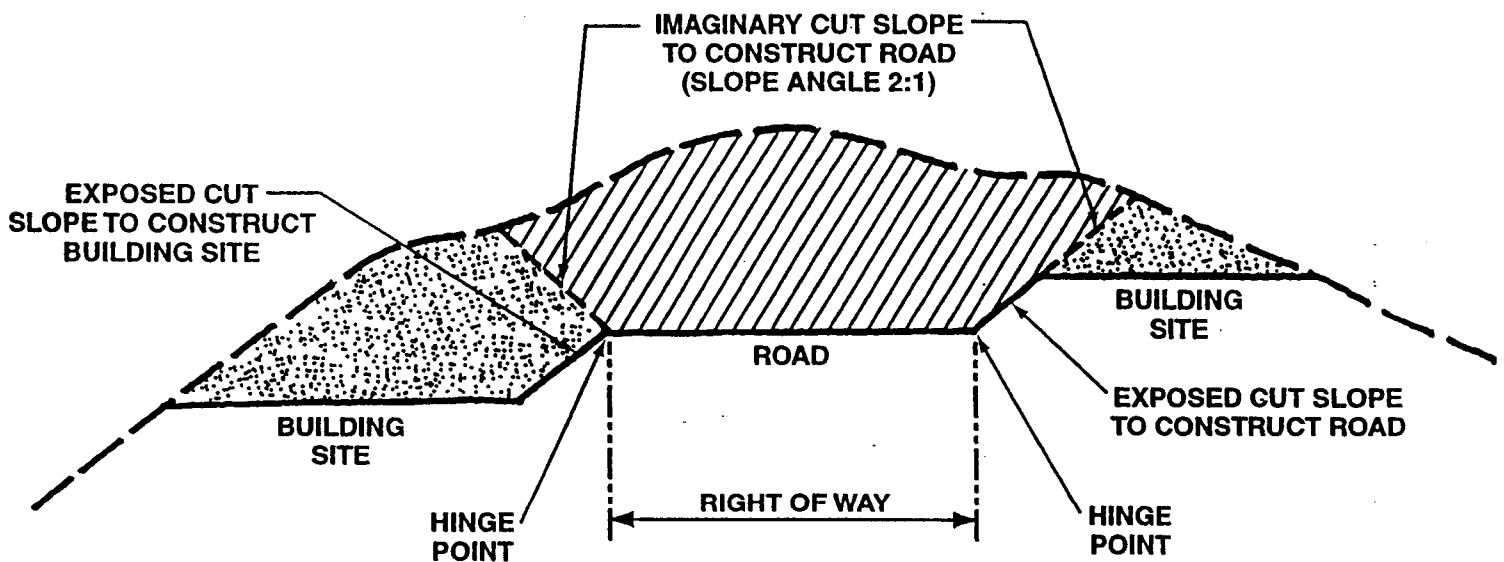
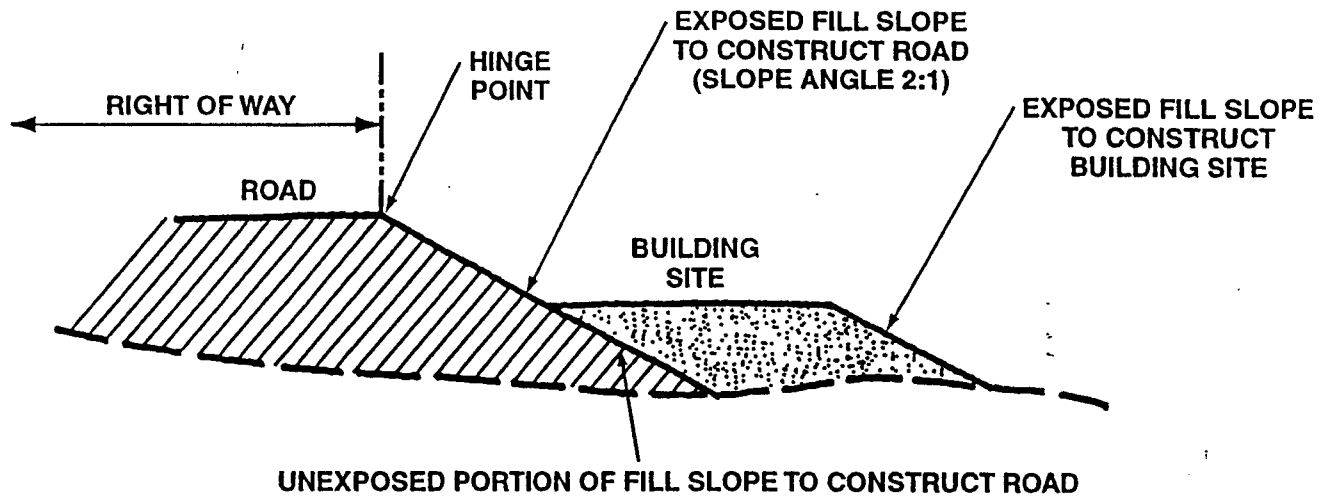


EXHIBIT 3
TYPICAL ROAD/DRIVEWAY AND SLOPE HEIGHT
CONDITIONS: ROAD/DRIVEWAY SERVING
MORE THAN 4 BUILDING SITES

SADDLE CREEK AREA PLAN



Grading required to construct a road or driveway to two or more building sites starts at the hinge point of road (each side) and proceeds at 2:1 slope angle, or any proposed slope angle steeper or flatter than 2:1 (eg. contour grading), either cut or fill, until slope daylight to natural grade (existing contours). For purposes of this calculation, construction of road grading assumes all pads do not exist, and excludes remedial grading.

Slope construction lines, including imaginary, exposed and unexposed are pitched at 2:1 for area plan level of grading analysis. At a later stage of plan refinement, slope pitches may deviate from 2:1 to enhance the project's rural character; preserve open space and/or achieve contour grading technique. Thus, the final grading analysis shall reflect any resultant changes in pad and road grading quantities and shall be in compliance with the grading standards set forth in this section.

LEGEND




-  GRADING FOR ROAD
-  GRADING FOR BUILDING SITE
-  ORIGINAL GROUND

EXHIBIT 4

ROAD/DRIVEWAY GRADING SERVING 2 OR MORE BUILDING SITES